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**PART**  
**#D30008/#D30009/#D30004/#D30005**

## Long Tube Headers, 2008-09 SRT8/5.7L Challenger

### PACKING LIST

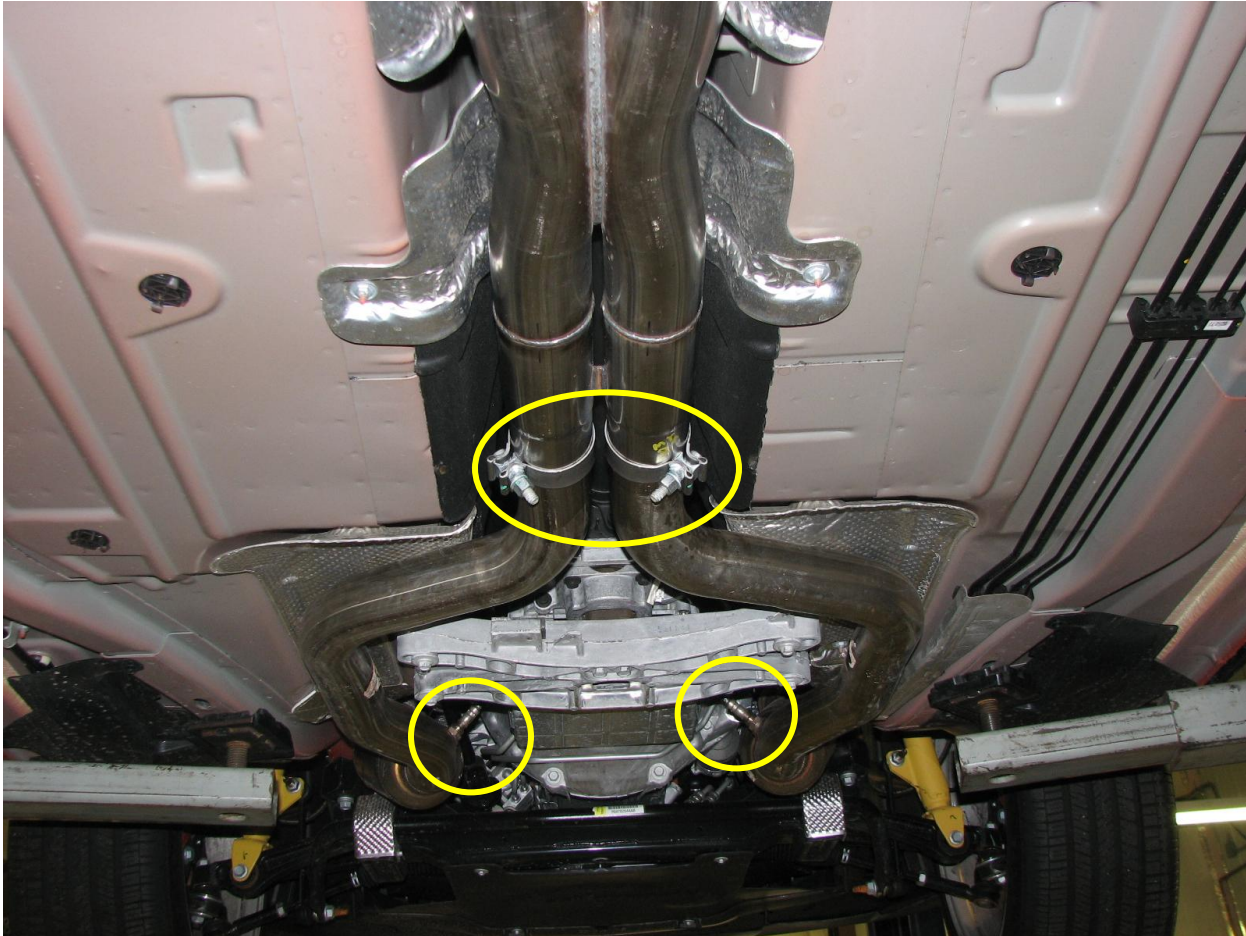
*Before installation, use this check list to make sure all necessary parts have been included.*

ITEM	QTY	CHECK	PART NUMBER	DESCRIPTION
1.	1	<input type="checkbox"/>	300417800	Header, Long Tube, Drivers Side SRT8
2.	1	<input type="checkbox"/>	300417801	Header, Long Tube, Passengers Side SRT8
3.	2	<input type="checkbox"/>	300362725	Gaskets, Manifold Flange, SRT8 Challenger/Charger
<b>Part 4 and 5 D30008 Only</b>				
4.	1	<input type="checkbox"/>	310602800	Down Pipe, SRT8, DS, Post Cat, Stock Cat-Back
5.	1	<input type="checkbox"/>	310602801	Down Pipe, SRT8, PS, Post Cat, Stock Cat-Back
<b>Part 6 and 7 D30009 &amp; D30004 Only</b>				
6.	1	<input type="checkbox"/>	310602803	Down Pipe, SRT8, DS, Post Cat, SLP Cat-Back
7.	1	<input type="checkbox"/>	310602804	Down Pipe, SRT8, PS, Post Cat, SLP Cat-Back
<b>Part 8 and 9 D30008 Only</b>				
8.	1	<input type="checkbox"/>	310602800	Down Pipe, SRT8, DS, Post Cat, Stock Cat-Back
9.	1	<input type="checkbox"/>	310602801	Down Pipe, SRT8, PS, Post Cat, Stock Cat-Back
<b>Part 10 and 11 D30005 Only</b>				
10.	1	<input type="checkbox"/>	310602805	Down Pipe, Challenger 5.7L, DS, Post Cat, Stock Cat-Back
11.	1	<input type="checkbox"/>	310602806	Down Pipe, Challenger 5.7L, PS, Post Cat, Stock Cat-Back
<b>PARTS 17-19 ARE IN D30004 ONLY</b>				
12.	2	<input type="checkbox"/>	310134001	Catalytic Converter, 3", Bullet, High Flow
13.	16	<input type="checkbox"/>	950080796	Bolt, M8-1.25X25mm Flange Head
14.	4	<input type="checkbox"/>	308940020	Torca Clamp, 3"
15.	2	<input type="checkbox"/>	020402100	Extension, Rear, 24", Charger/Challenger 05-08
16.	2	<input type="checkbox"/>	020402102	Extension, Front, 12" Challenger 2008
17.	2	<input type="checkbox"/>	700570000	8mm - 1.25 mm nut, class 10
18.	1	<input type="checkbox"/>	940790000	M8x1.25 x 80mm - 50mm/11mm stud
19.	1	<input type="checkbox"/>	970768000	spacer, 3/8"x 1-1/4 steel
20.	1	<input type="checkbox"/>	*****	Instructions

**WARNING: SLP Recommends allowing the vehicle to cool (not running) for five hours before beginning installation.**

**INSTALLATION INSTRUCTIONS – #D30008/D30009/D30004/D30005**

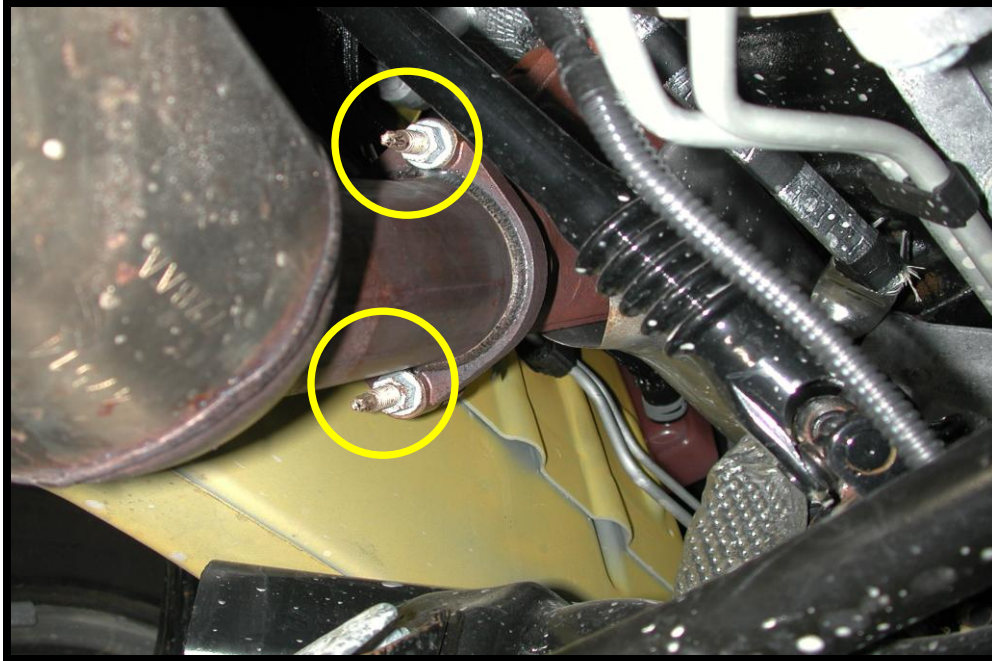
- 1 Disconnect the negative battery terminal (battery located in trunk).
- 2 Loosen the two band clamps on the stock cat-back as shown below. Remove the cat-back exhaust system (SLP and factory are removed identically) and the rear oxygen sensors, see photo 1 on the next page.



**Photo 1: Removal of Cat-Back and Rear Oxygen Sensors**

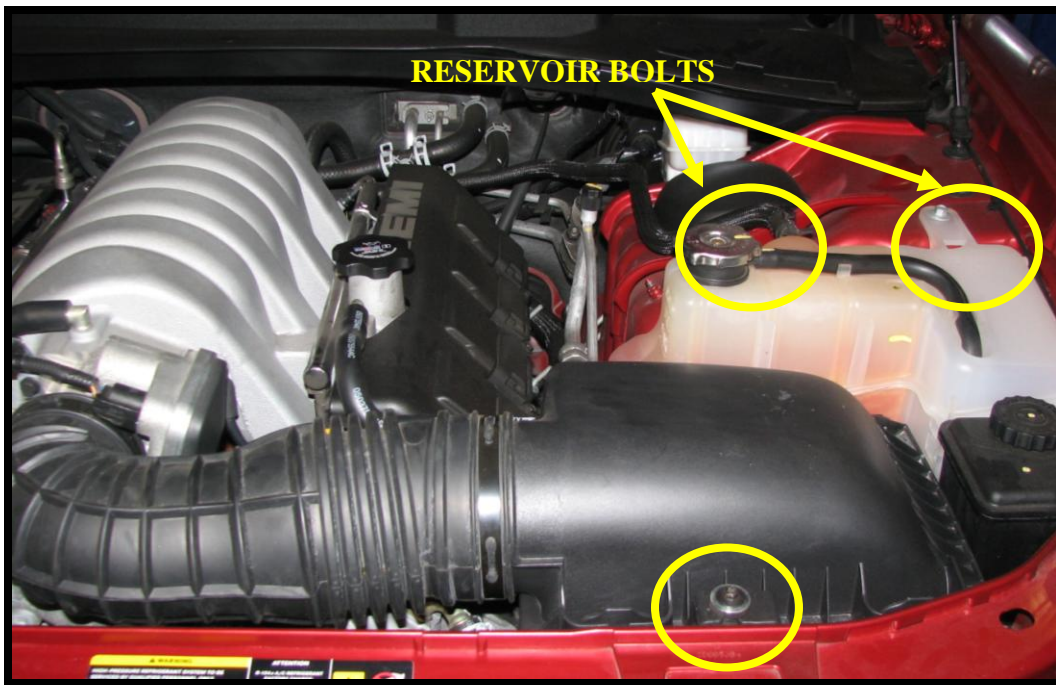
- 3 Next remove the two nuts with springs attached on the driver's side stock muffler assembly. Carefully remove the rear driver's side muffler assembly from the vehicle by sliding the rubber isolators off the hangers attached to the assembly.
- 4 Next, with the aid of a helper remove the remaining stock exhaust system as one piece. Removing all remaining rubber isolators from the stock exhaust system and carefully sliding the exhaust off of the stock CAT down-pipes.
- 5 Remove splash guard under front cross member.

- 6 Remove the front O2 sensors from each manifold. Then using a 16mm socket, remove 2 nuts per side and remove header down pipes see photo 2 on the next page.



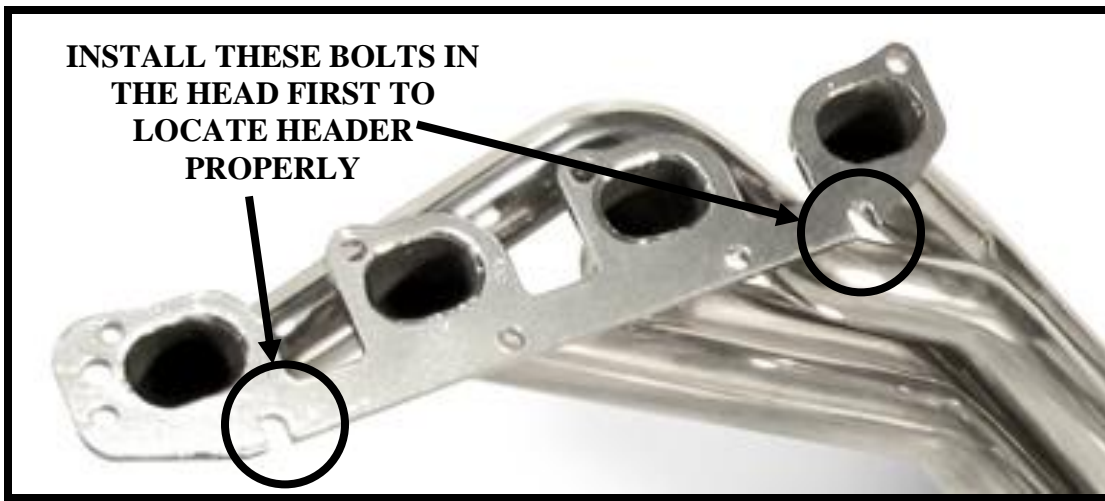
**Photo 2: Removal of Downpipes from Factory Manifold (5.7L Hemi Shown)**

- 7 Move to the engine bay and remove the factory air box and intake tube. Next, remove the two 10mm bolts from the coolant overflow tank. Push the tab on the power steering reservoir and lift the coolant reservoir out of the way. This will allow better access to the driver side manifold bolts. See photo below 3 below.



### **Photo 3: Coolant Reservoir and Intake Tube**

- 8 To gain access to the passenger side bolts, remove the oil filter. The dipstick can be removed for additional clearance by removing the two nuts on passenger side valve cover.
- 9 Using a 13mm socket/wrench, remove the manifolds and stock gaskets. SLP found access to the manifolds bolts was best by working from underneath the vehicle and from the rear of the engine. A series of extensions and universal joints may prove useful.
- 10 Before installing the SLP Long Tube Headers, plug in both front (12" Length) and rear (24" Length) O2 extension harnesses to the factory harness.
- 11 Using the supplied header bolts, install bolts on the bottom most outward holes first. They need to be installed loosely, approximately 5-6 threads. Next, place the new gasket on these bolts with the slots down and the gasket resting on the new bolts. Install the header by lifting it into place from the underneath the vehicle and sliding the flange onto the previously installed bolts. Install the remaining bolts. The header should fit into place with very little effort and is done for both sides identically. SLP found access to the long tube bolts was best by working from underneath the vehicle and from the rear of the engine. A series of extensions and universal joints may prove useful. See photo 4 below.



**Photo 4: SRT8/5.7L Header Flange**

- 12 Slide a 3” band clamp onto the large end of the SLP catalytic converter and fit over the header collector. Leave clamps loose at this point. Next slide 3” band clamp over large end of the new SLP down pipe and install over the outlet of the SLP cat.
- 13 With the clamps still loose, fit the cat-back exhaust system (SLP and factory re-install identically) back onto the vehicle. Re-install brace on tunnel.
- 14 Working from the front of the vehicle towards the back, tighten each exhaust clamp while maintaining alignment of the exhaust system. Be sure clearance to tunnel brace, drive-train, and body parts is adequate. Adjust as needed.
- 15 Reinstall the oil filter and re-fill oil as needed. Also reinstall coolant tank, power steering reservoir and air box if removed. Then install the front and rear oxygen sensors and connect the extensions to the previously installed oxygen sensor extensions. Finally reconnect negative battery terminal.

### **FOR 5.7L Challenger ONLY**

- 16 Passenger side header: Follow the same procedure described for the driver side but use the supplied stud in the forward most upper hole. This will allow the dipstick to be mounted to the engine. Once header is in place, tighten the first nut over the stud. Install the remaining header bolts and tighten from inside to outer bolts. Use the supplied spacer and nut to mount the dipstick tube back on the engine.
- 17 The installation is now complete! ENJOY!